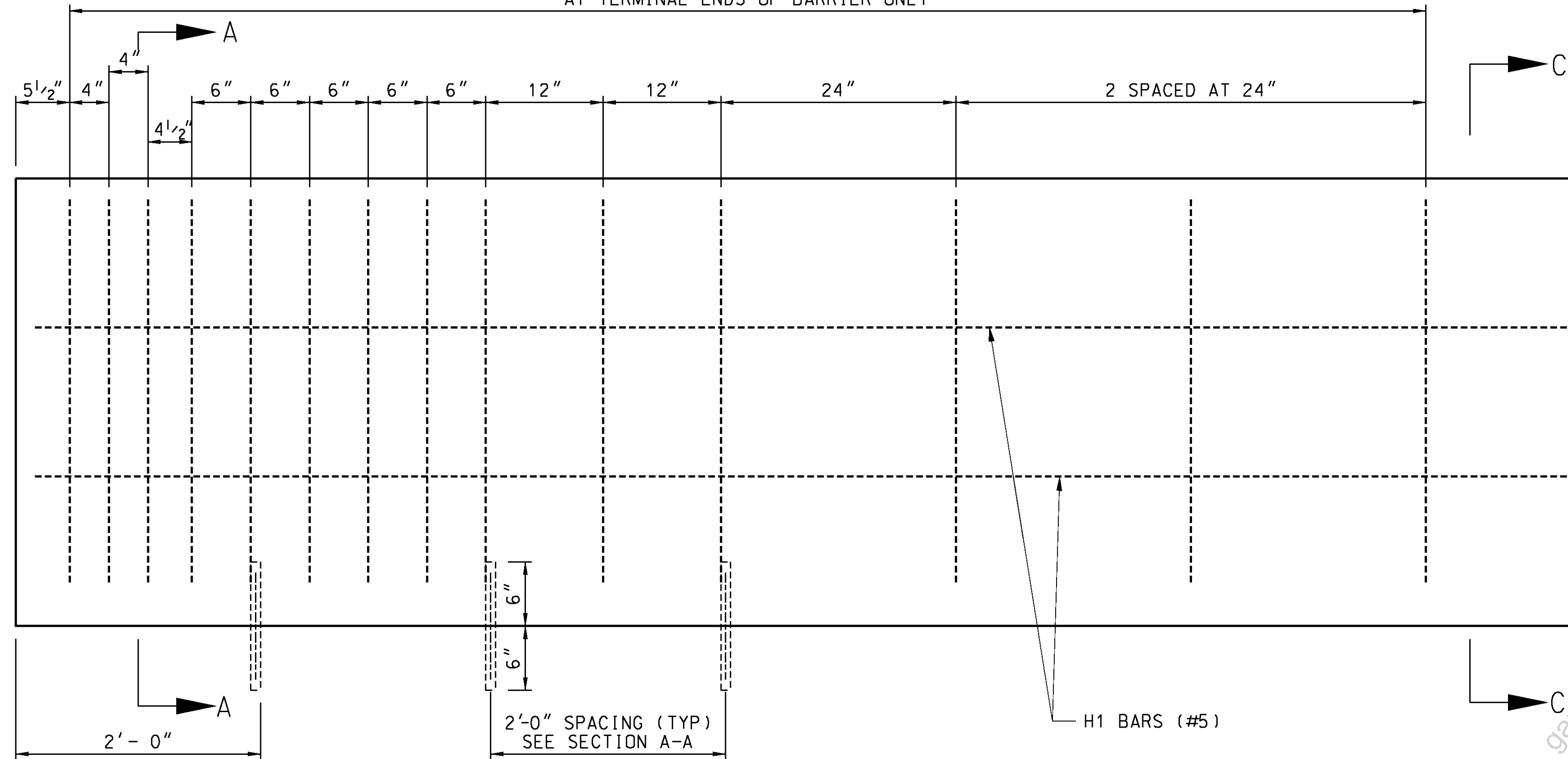


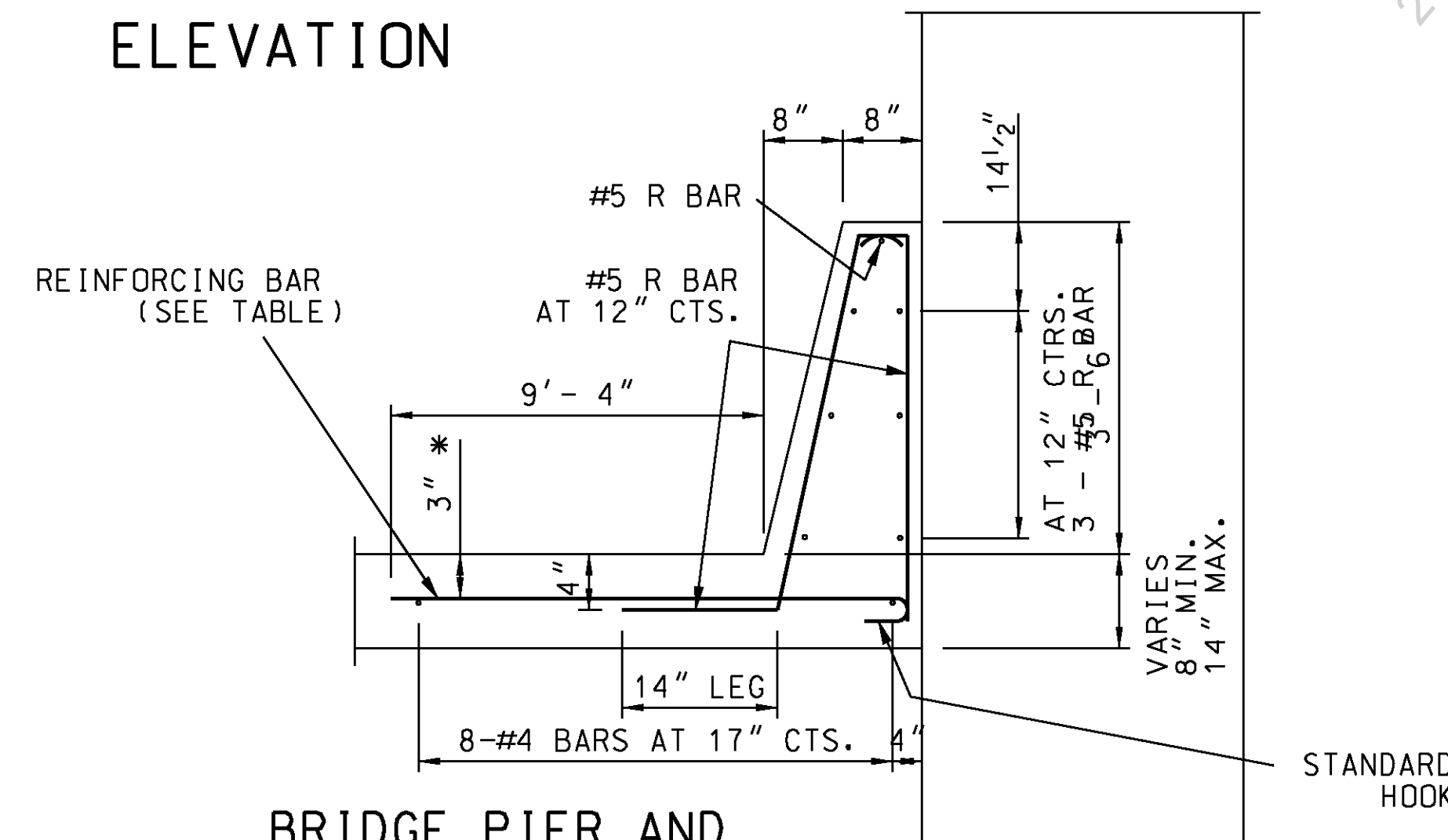
PLAN VIEW
(SYMMETRICAL ABOUT CENTER LINE)

LIMITS OF #4 - V1 SPACED AS SHOWN BELOW
AT TERMINAL ENDS OF BARRIER ONLY



ELEVATION

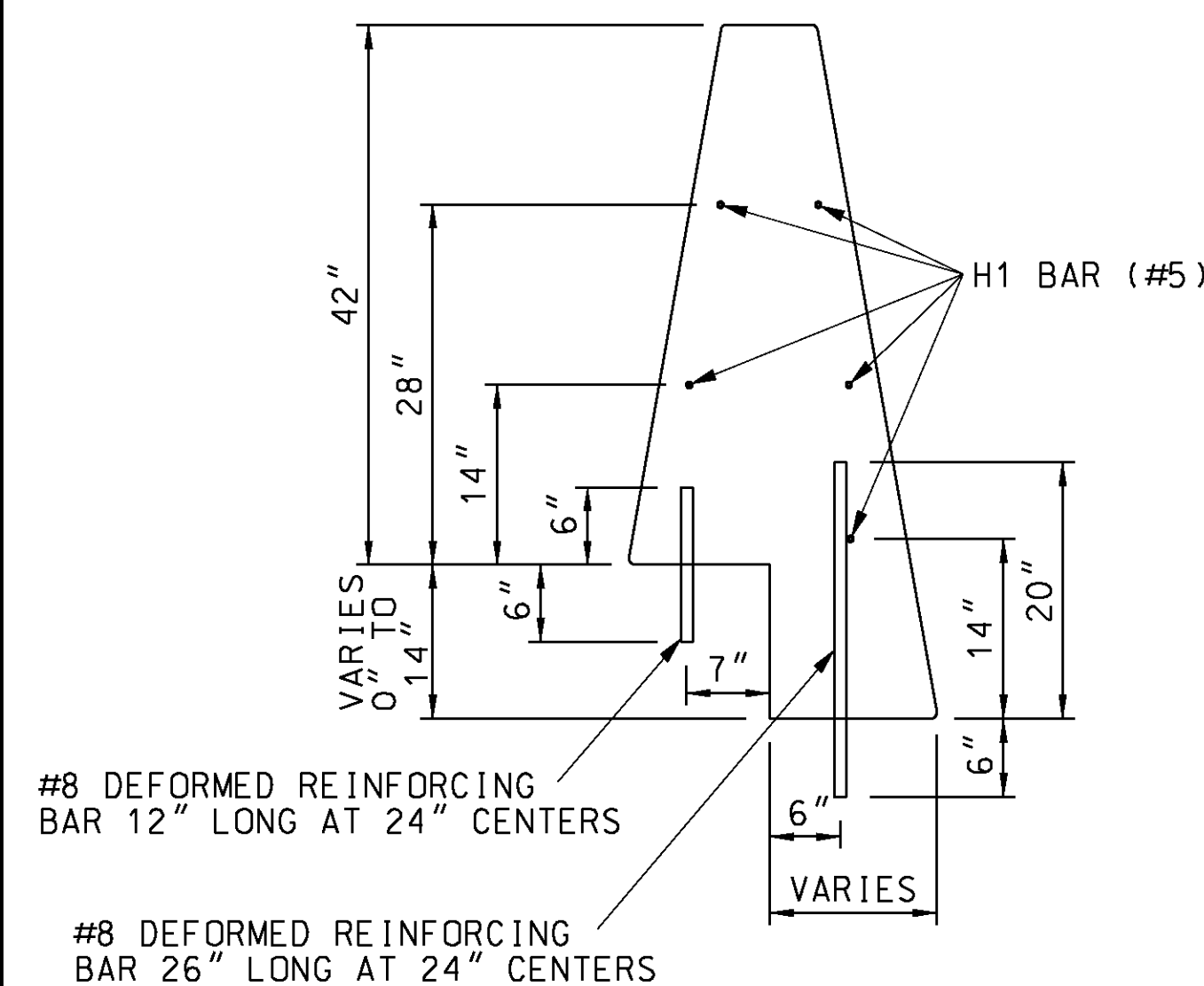
TABLE OF TRANSVERSE PAVEMENT REINFORCEMENT		
SLAB DEPTH	BAR SIZE	SPACING
8"	#6	6"
9"	#6	8"
10"	#6	9"
11"	#5	8"
12"	#5	9"
13"	#5	10"
14"	#5	11"



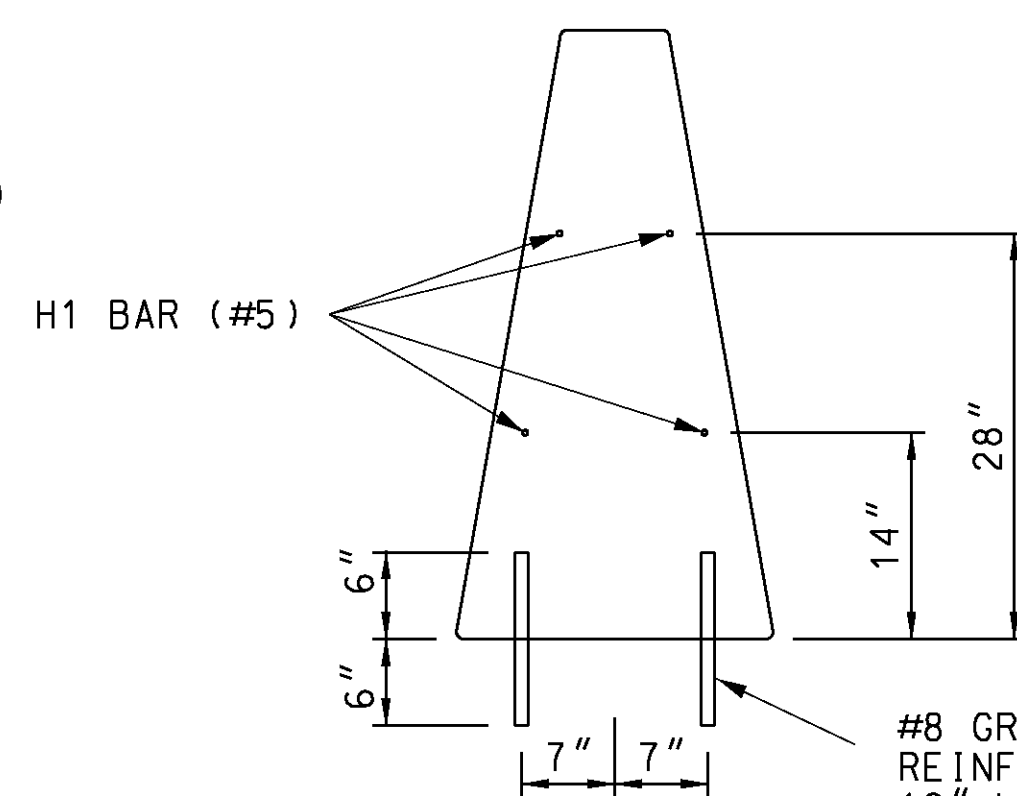
BRIDGE PIER AND MSE WALL PROTECTION *

* USED IN ANY CASE WHERE INTEGRAL BARRIER PROTECTION IS NEEDED.

3/8" BEVEL, 1/2" RADIUS OR ALTERNATE AS APPROVED BY THE ENGINEER

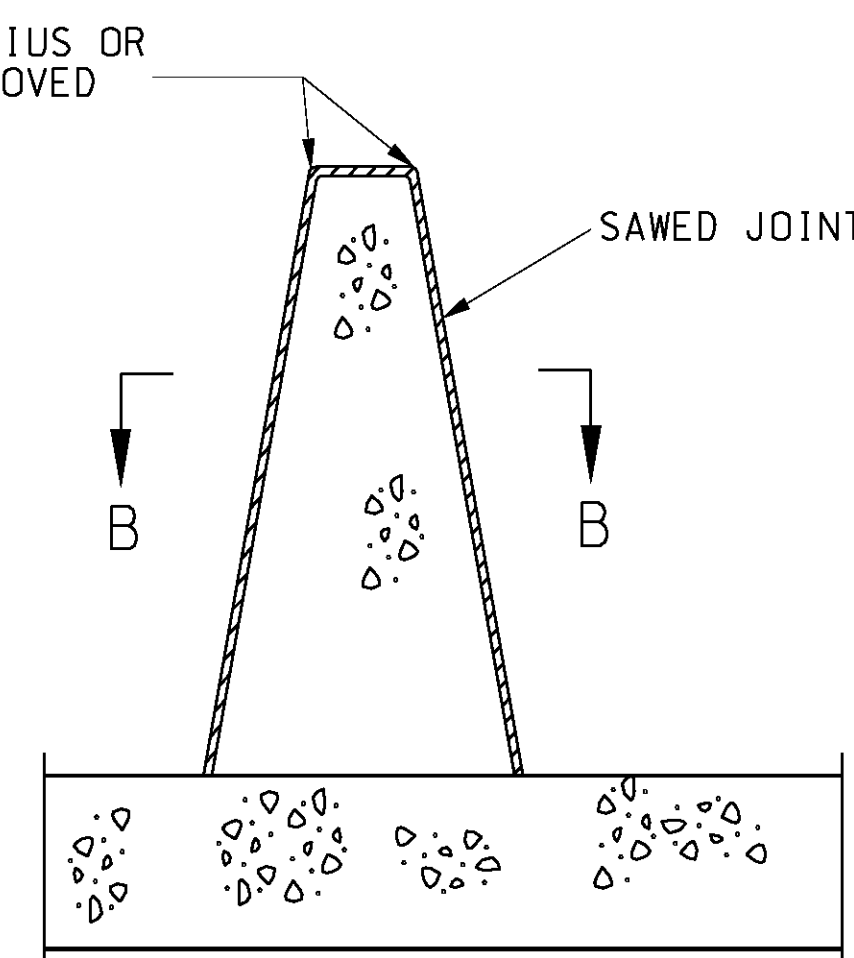


SECTION C-C
(STEPPED PAVEMENT)

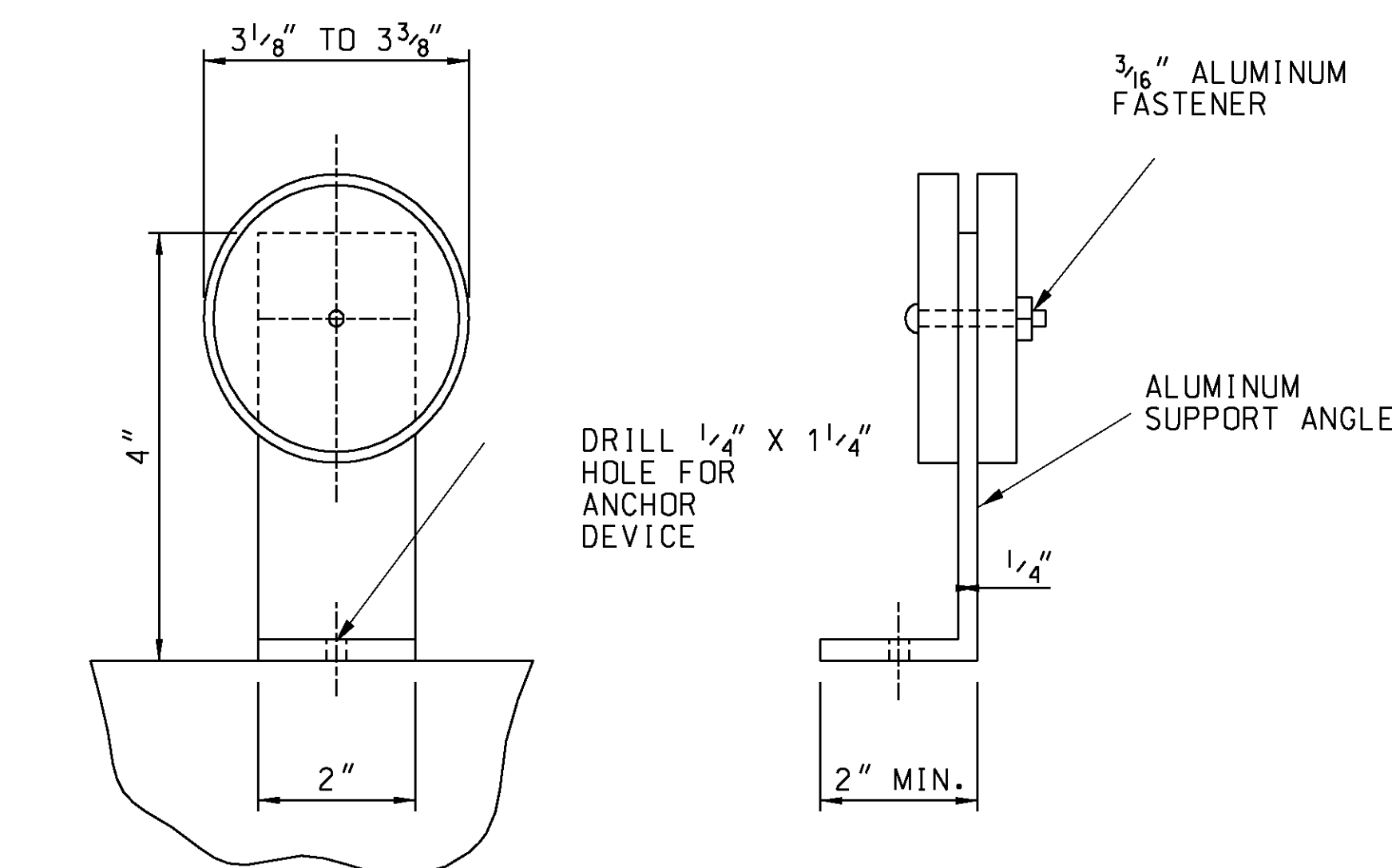


SECTION C-C

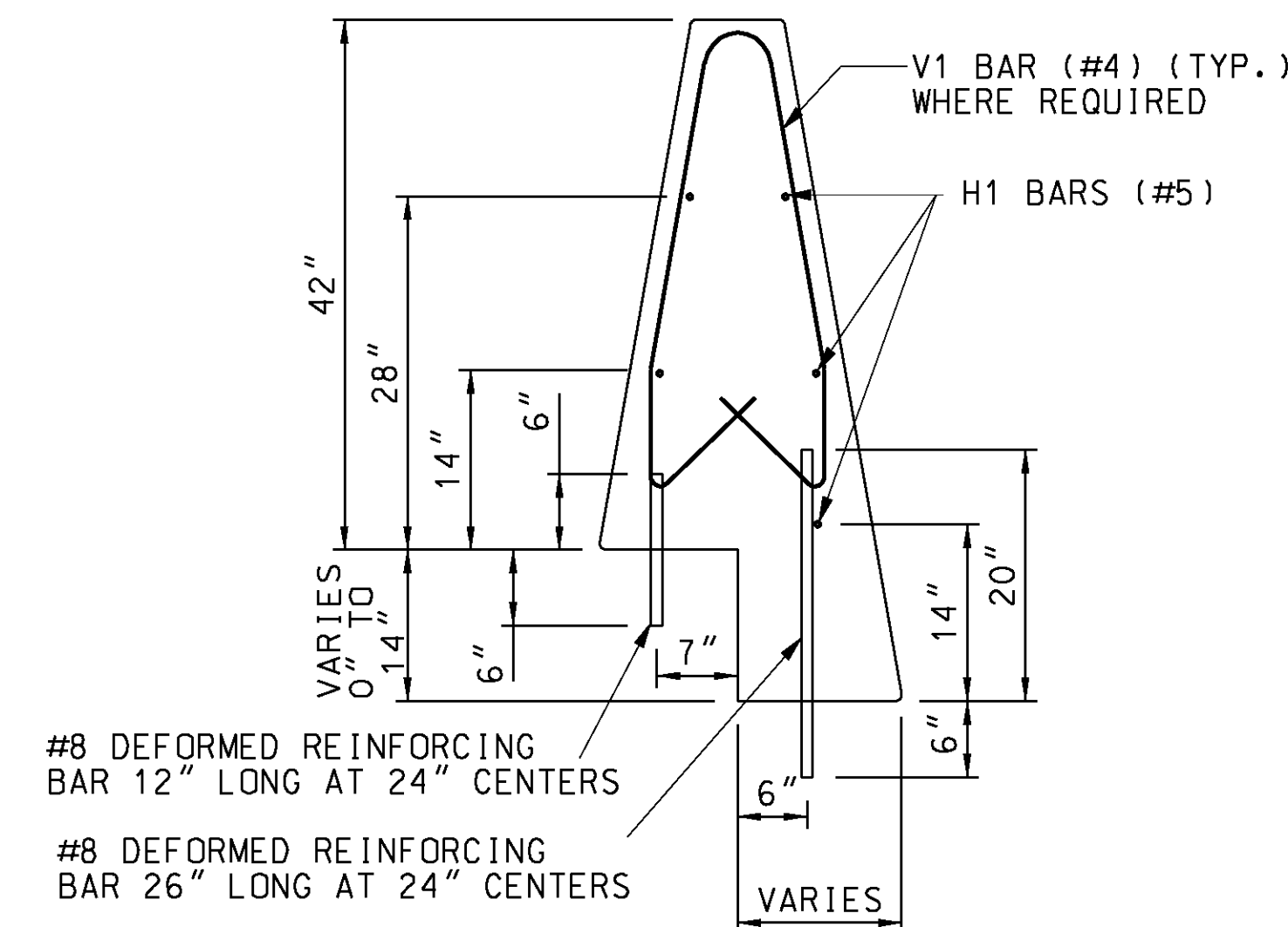
SECTION THROUGH SAWED JOINT



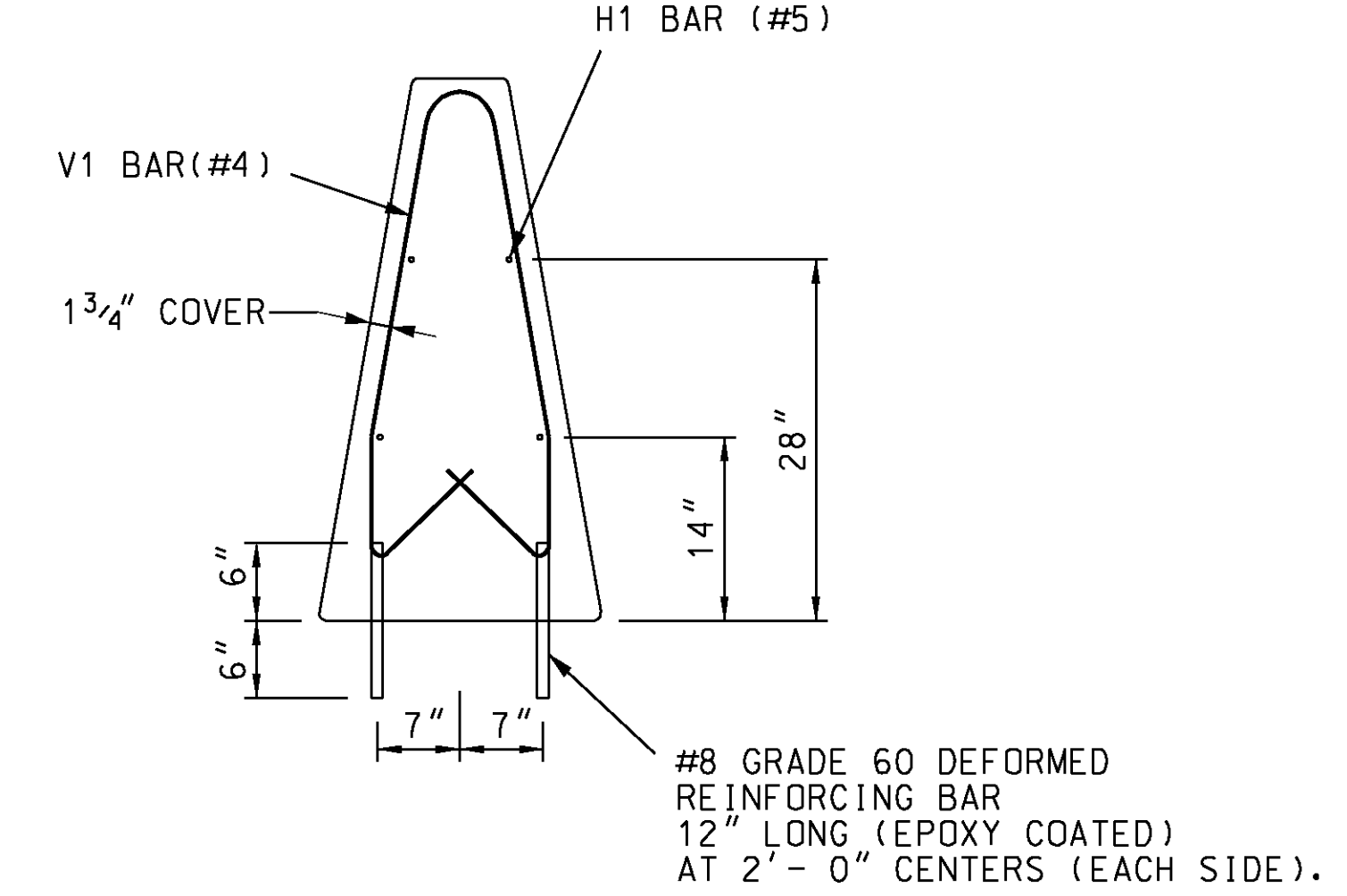
SECTION B-B



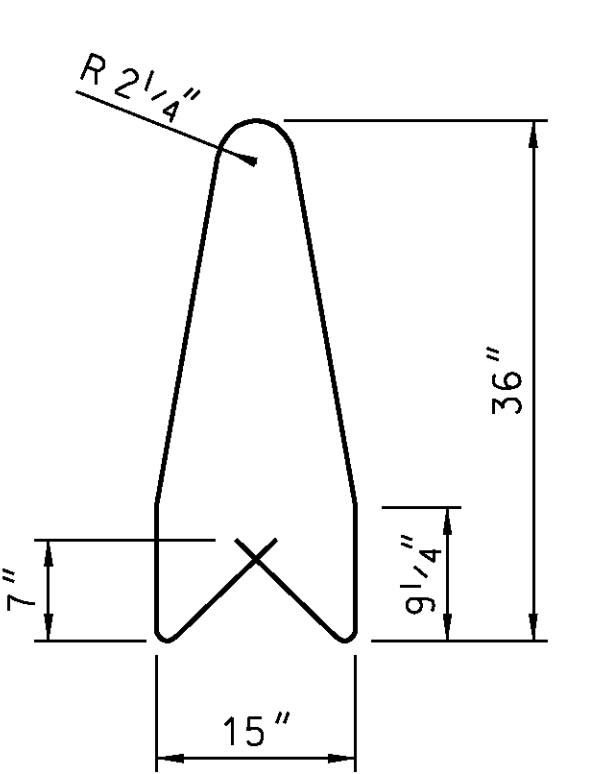
DOUBLE DELINEATOR



SECTION A-A
(STEPPED PAVEMENT)



SECTION A-A
(NORMAL PAVEMENT)



V1 BAR (#4)

REINFORCING DETAILS

GENERAL NOTES

ALL CONCRETE AND REINFORCING STEEL SHALL CONFORM TO SECTION 617 OF THE MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

CHAMFER TOP AND END EDGES 3/4 INCH.

BAR SPLICES FOR ROADWAY BARRIER SHALL BE A MINIMUM OF 24 TIMES THE NOMINAL DIAMETER OF THE BAR.

ANY METHOD DEvised BY THE CONTRACTOR AND APPROVED BY THE ENGINEER THAT WILL ASSURE THE LONGITUDINAL ROADWAY STEEL WILL BE POSITIONED $\pm 1/2$ INCH AS DIMENSIONED WILL BE SATISFACTORY.

THE CONTRACTOR HAS THE OPTION TO SLIP-FORM THE BARRIER, IN WHICH CASE, ADDITIONAL REINFORCEMENT MAY BE TIED TO THE UPPER TWO-THIRDS OF THE REINFORCING CAGE TO PROVIDE BRACING.

THIS BARRIER SHALL NOT BE USED TO SUPPORT HIGHWAY LIGHTING POLES.

THIS BARRIER IS NOT TO BE USED FOR BRIDGE ROADWAY APPLICATIONS.

DELINEATORS TO BE MOUNTED ON MEDIAN BARRIERS SHALL BE YELLOW WHERE THE BARRIER IS ON THE LEFT IN THE DIRECTION OF TRAFFIC; SHALL BE WHITE WHERE THE BARRIER IS ON THE RIGHT IN THE DIRECTION OF TRAFFIC; AND SHALL BE DOUBLE YELLOW WHERE THE BARRIER IS IN THE MEDIAN WITH TRAFFIC IN EACH DIRECTION.

ALL MATERIALS FOR SUPPORTING DELINEATORS SHALL BE ALUMINUM EXCEPT ANCHOR DEVICES.

ANCHOR DEVICES SHALL BE EITHER ALUMINUM OR GALVANIZED STEEL AND SHALL BE EXPANSIVE BOLTS OR ADHESIVE SELF-GROUTING BOLTS. IF GALVANIZED STEEL BOLTS ARE USED, A NON-CONDUCTIVE WASHER SHALL BE USED BETWEEN THE ALUMINUM ALLOY SUPPORT ANGLE AND THE HEAD OF THE BOLT.

DELINEATORS SHALL HAVE A MAXIMUM OF 100' SPACING FOR A 0* TO 2*45' CURVE AND 60' MAXIMUM SPACING FOR A 3* TO 6* CURVE.

NO DIRECT PAYMENT IS MADE FOR DELINEATORS.

SAWED JOINTS SHALL BE LOCATED AT PAVEMENT TRANSVERSE JOINTS.

ALL REINFORCING STEEL SHALL BE GRADE 60 EPOXY-COATED IN ACCORDANCE WITH SECTION 1036 OF THE MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NO DIRECT PAYMENT WILL BE MADE FOR ALL REINFORCING STEEL.

NO DIRECT PAYMENT WILL BE MADE FOR FURNISHING, GROUTING AND INSTALLING #8 BARS AND SAWING OF JOINTS.

#8 REINFORCING BARS TO BE INSTALLED INTO PAVEMENT IN ACCORDANCE WITH SECTION 1039.3 OF THE MISSOURI STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION			
CONCRETE TRAFFIC BARRIER			
TYPE C			
DATE: _____	EFFECTIVE: 01-01-2001	617.10A	1/1