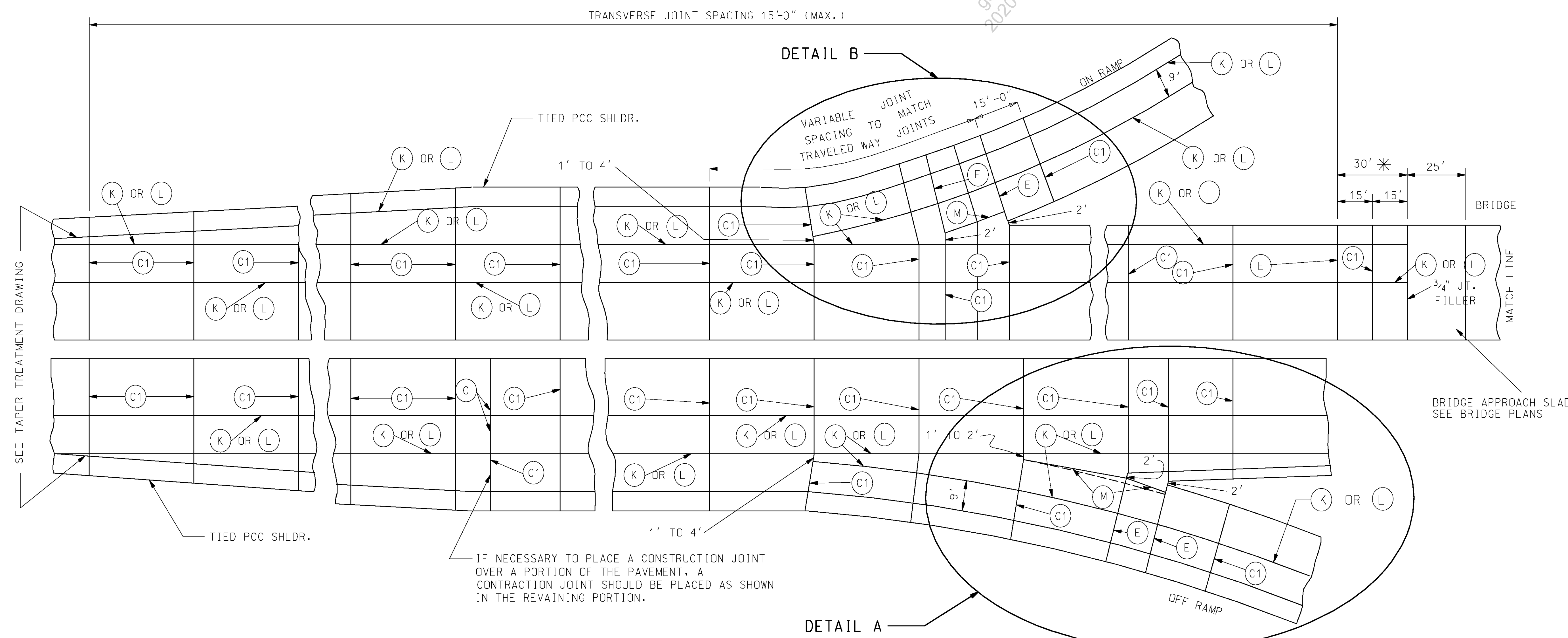
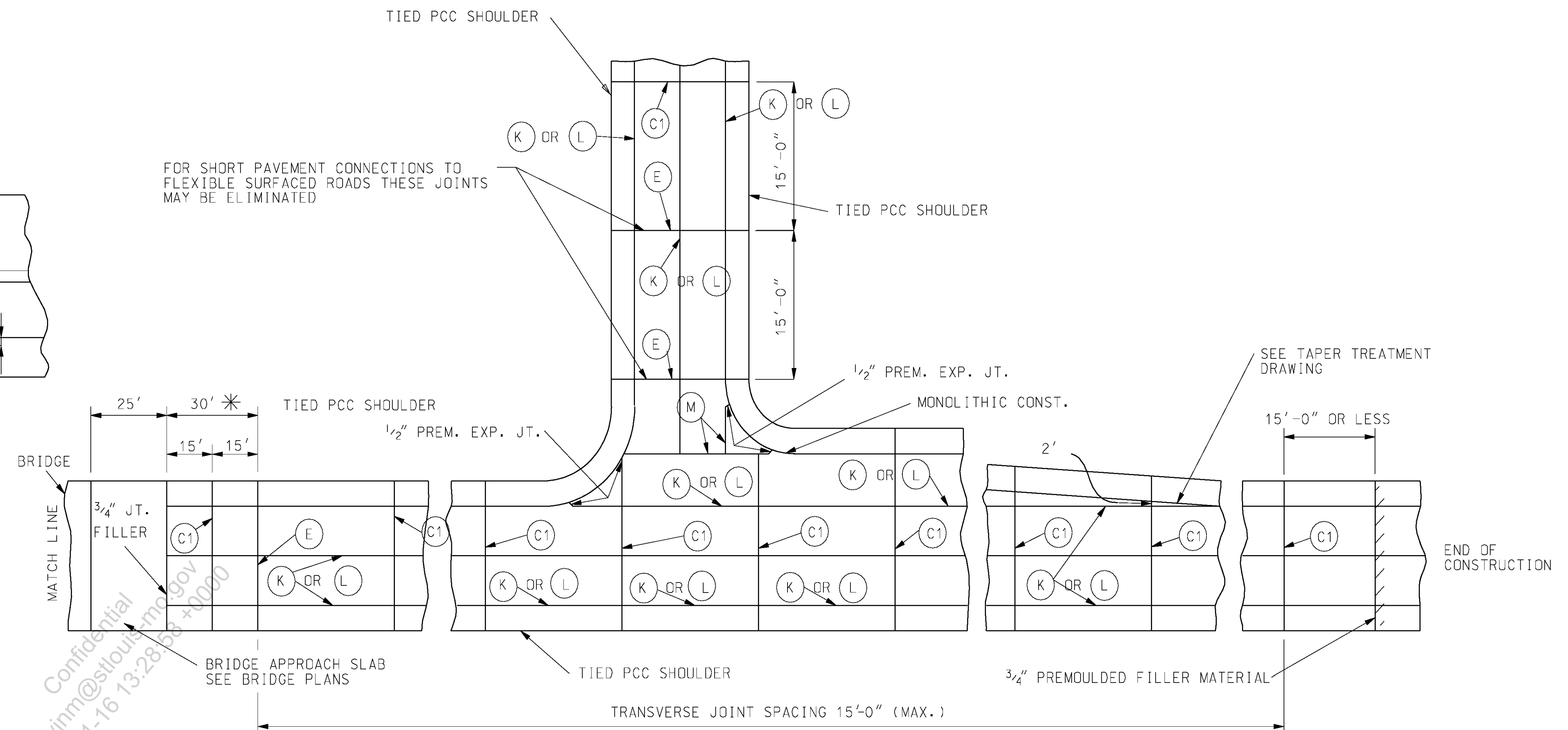
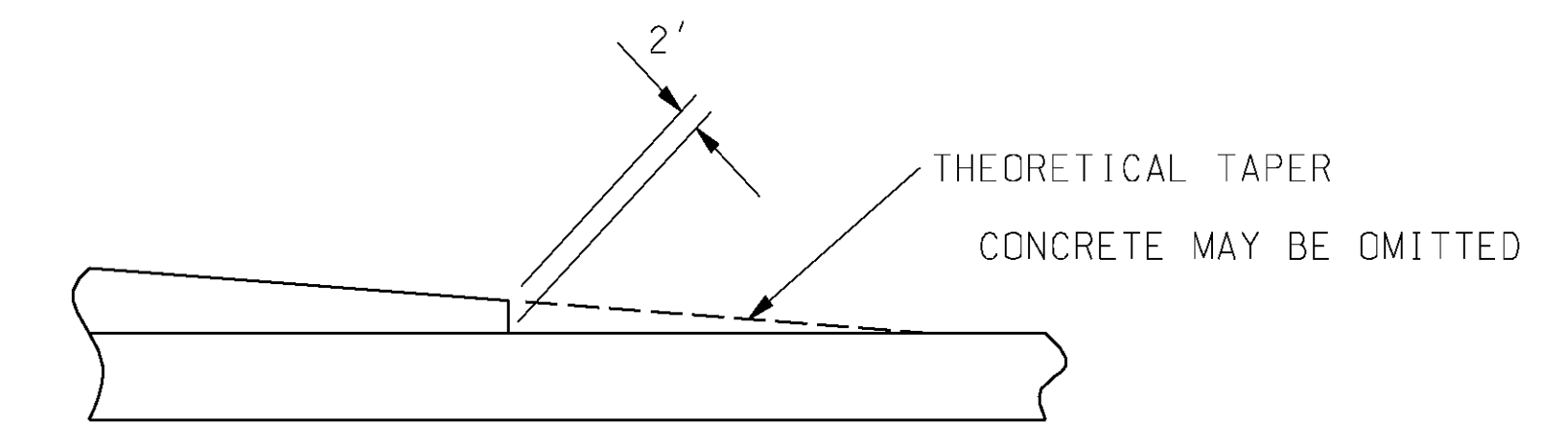


\*PCC APPROACH PAVEMENT



#### GENERAL NOTES:

THE CONTRACTOR SHALL DETERMINE THE PAVING WIDTH. (K) JOINTS SHALL BE USED BETWEEN ADJOINING SECTIONS OF PAVEMENT. (L) JOINTS SHALL BE USED WITHIN THE PAVING WIDTH.

FOR JOINT DETAILS, SEE SHEET NO. 2.

THE JOINT LAYOUT OF RAMPS IS TYPICAL FOR OUTER RAMPS OF CLOVERLEAF AND DIAMOND INTERCHANGES. SEE OTHER DRAWINGS FOR SPECIAL JOINT LAYOUTS.

JOINT SPACING SHOWN IS MAXIMUM AND IS TO BE REDUCED TO AVOID CONFLICT WITH ABUTTING STRUCTURES. JOINTS IN MULTI-LANE PAVEMENT ARE TO BE CONTINUOUS.

MISSOURI HIGHWAY AND TRANSPORTATION COMMISSION

CONCRETE PAVEMENT AND  
BASE APPURTENANCES FOR  
15 FT. JOINT SPACING

DATE: \_\_\_\_\_

EFFECTIVE: 04-01-2000

502.05D

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