



2024

Annual Noise and Aircraft Traffic Report



Photo by Gerald McGrath

STL 2024 Annual Noise and Aircraft Traffic Report

This report contains aircraft traffic and noise complaints data for the year 2024. It provides a summary of St. Louis Lambert International Airport's operations and noise complaints during 2024. The National Airspace System data in this report is compiled and provided by PASSUR Aerospace, Inc. This report is illustrative of trends and distribution and should not be construed as official FAA traffic counts.

2024

Noise Complaints

Ninety one (91) total noise complaints were received in 2024. Forty eight (48) of those noise complaints were submitted by a single individual located in zip code 63301. An individual located in zip code 63134 submitted a total of fifteen (15) complaints. A individual located in 63044 submitted five (5) complaints. The remaining twenty three (23) complaints were received from fifteen (15) respective households located in the zip codes shown in the table below, which households each submitted between one (1) and three (3) separate complaints.



Quarter	Zip	Complaints	Zip	Complaints								
1st	63043	1	63114	1	63301	3						
2nd	63044	1	63114	1	63130	2	63134	2	63301	7		
3rd	63114	4	63121	5	63134	8	63301	19				
4th	63044	5	63116	1	63121	1	63134	11	63301	19		

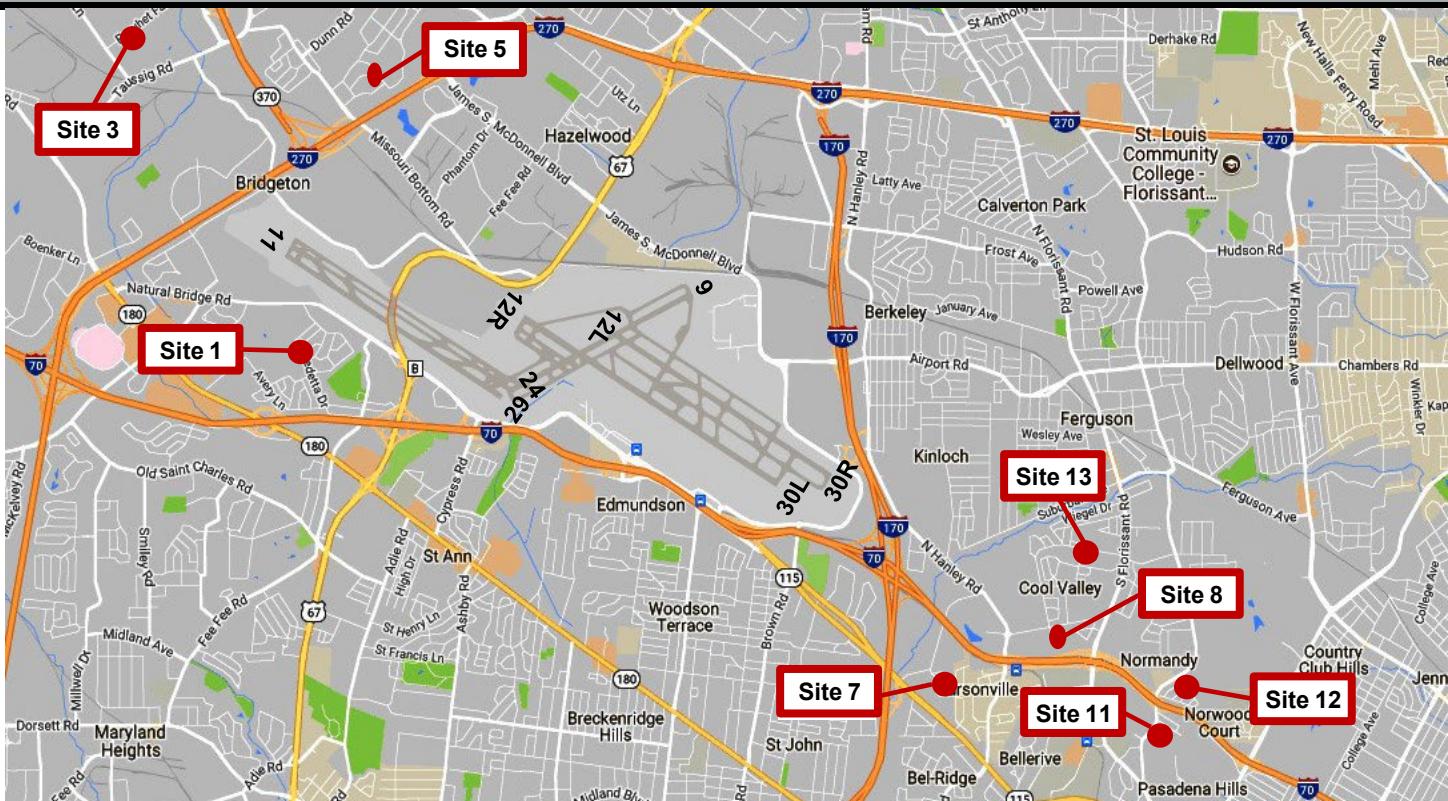
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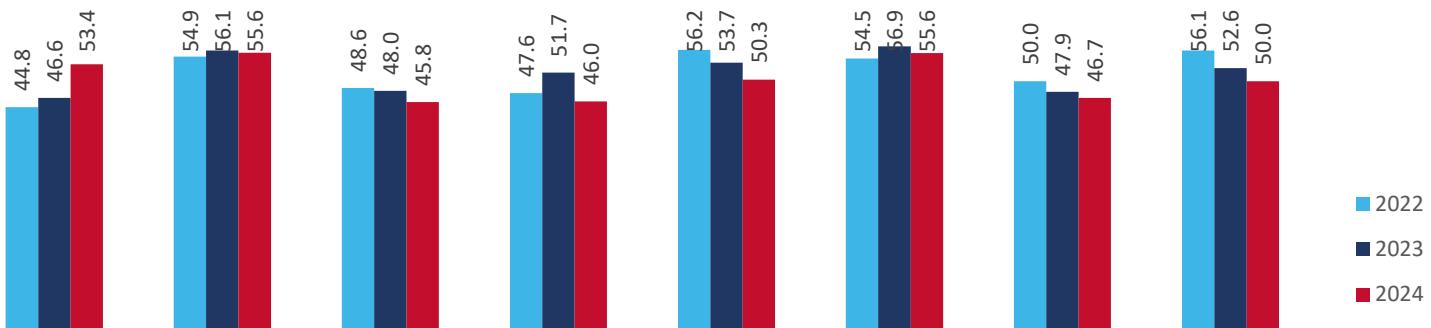
RUNWAY IDENTIFICATION

The runways at St. Louis Lambert International Airport are 6/24 (7,603'x150'), 11/29 (9,000'x150'), 12L/30R (9,013'x150'), and 12R/30L (11,020'x150')

PERMANENT NOISE MONITORING SITES



The DNL (Day-Night average sound level) metric describes the total aircraft noise exposure during a given period. In computing DNL, an extra weighting of 10dB is assigned to any aircraft noise event occurring between the hours of 10 pm to 7 am. The FAA has established 65 DNL as the threshold above which aircraft noise is considered incompatible with residential use. Note: STL's sound level meters are calibrated annually. Faulty equipment at Site 11 reported inaccurate sound level measurements for aircraft, community, and total noise during the 2nd half of 2024. Therefore, the 2024 DNL indicated below for Site 11 is derived exclusively from January-May data.



Benedetta, Bridgeton	Prouhet Farm, Bridgeton Athletic	Ville Rosa @ Auriesville, Hazelwood	Belfore @ Loganberry, Bel-Ridge	James, Cool Valley	North Hills @ Country Club, Pasadena Pk	Bellwood @ Kirkland, Normandy	Brotherton @ North Hills, Ferguson
Site 1	Site 3	Site 5	Site 7	Site 8	Site 11	Site 12	Site 13

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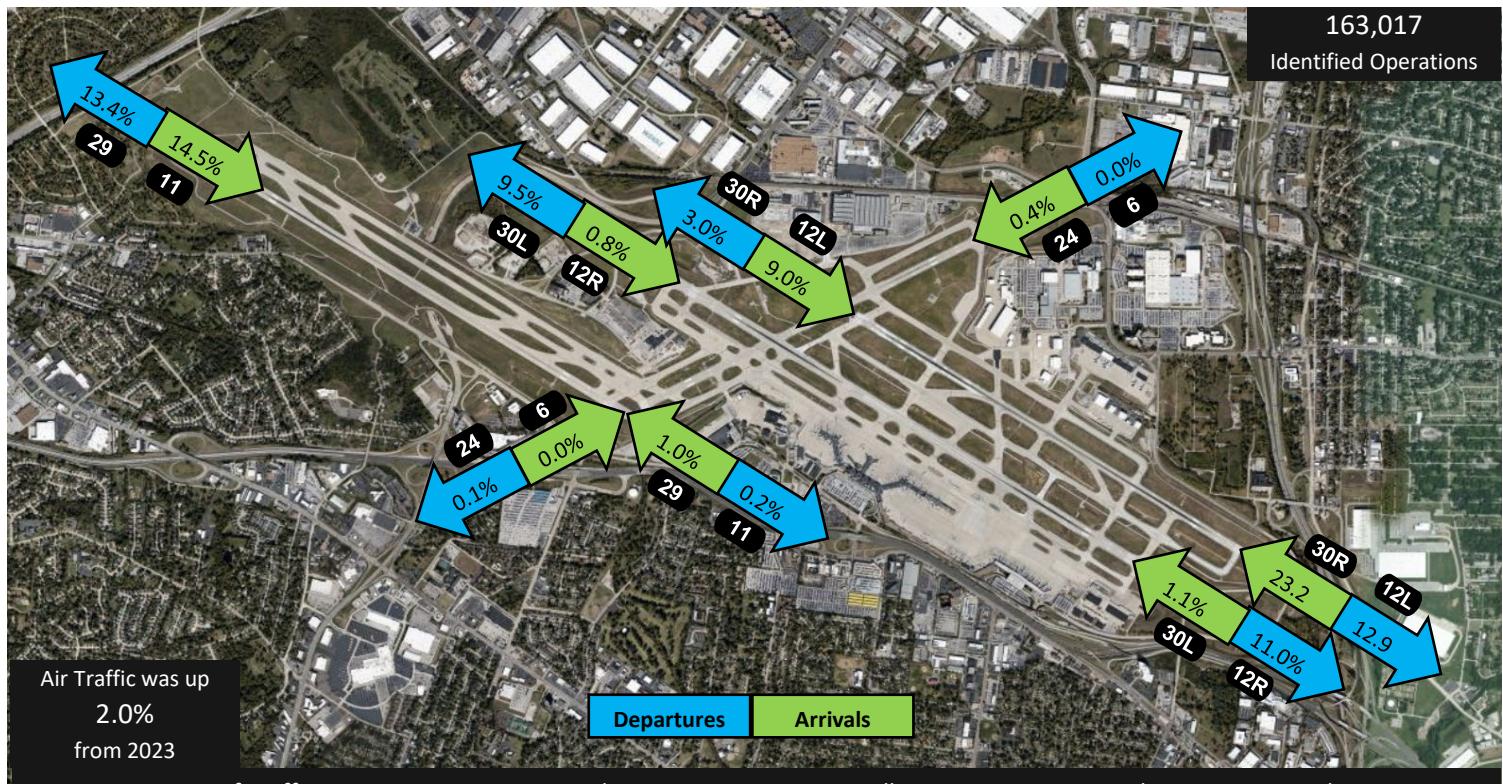
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DISTRIBUTION OF TRAFFIC

There were 163,892 total operations recorded in 2024 -- an increase of 2% from 160,647 operations in 2023. The chart below shows the total utilization for all runways during the year. The most heavily utilized runway for takeoffs was 29, which had 21,767 operations (26.5%). Runway 30R recorded the most landings, 37,737 (46.1%). The diagonal Runway 6/24 was the least used runway for the year, with only a combined 904 takeoffs and landings, representing just over .6% of all operations. The distribution of traffic (East Flow vs. West Flow) for 2024 was 48.1% East Flow, and 51.3% West Flow.

*The Passur Aerospace Noise and Operations Monitoring System (NOMS) collects FAA air traffic data from the National Airspace System (NAS) and permanent noise monitoring data and correlates flight tracks, noise monitoring events and noise complaints. The NOMS provides illustrative trends and should not be construed as official traffic counts.

2024 Total



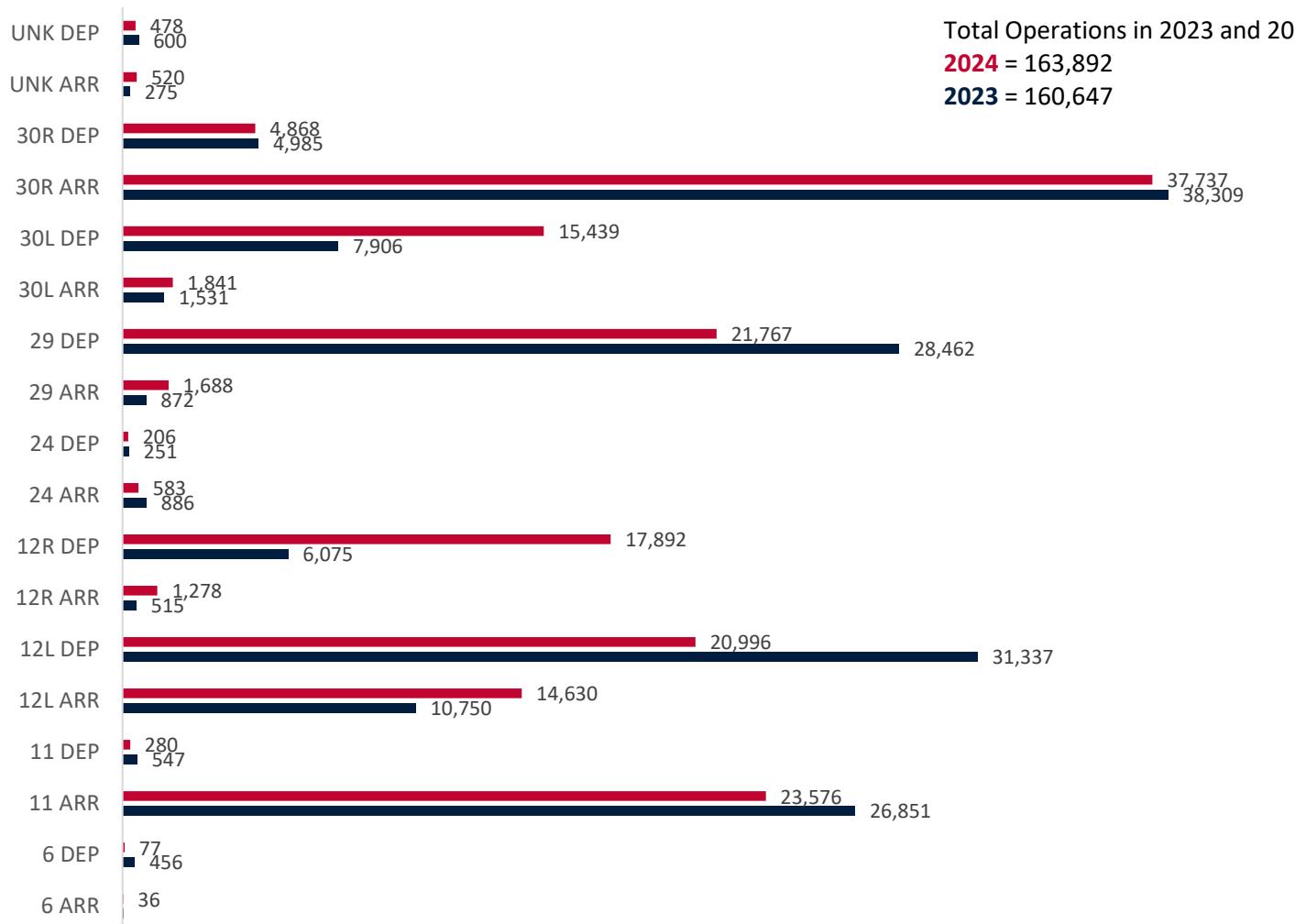
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OPERATIONS BY RUNWAY

There were 163,892 operations in 2024, compared to 160,647 operations in 2023. That represents an increase of 2% according to the PASSUR Aerospace data*. Runway 30R was the most utilized runway for known arrivals, with 37,737 (46.1%), followed by Runway 11, with 23,576 (28.8%). For known departures, Runway 29 with 21,767 (26.5%), followed by Runway 12L with 20,996 (25.6%) were most utilized. During 2024 there were 163,033 (99.5%) operations for which the runway was identified (known) and 859 (0.5%) operations for which the runway was not identified (unknown) by PASSUR Aerospace.

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Yearly Operations by Runway 2023 and 2024



■ 2024 ■ 2023

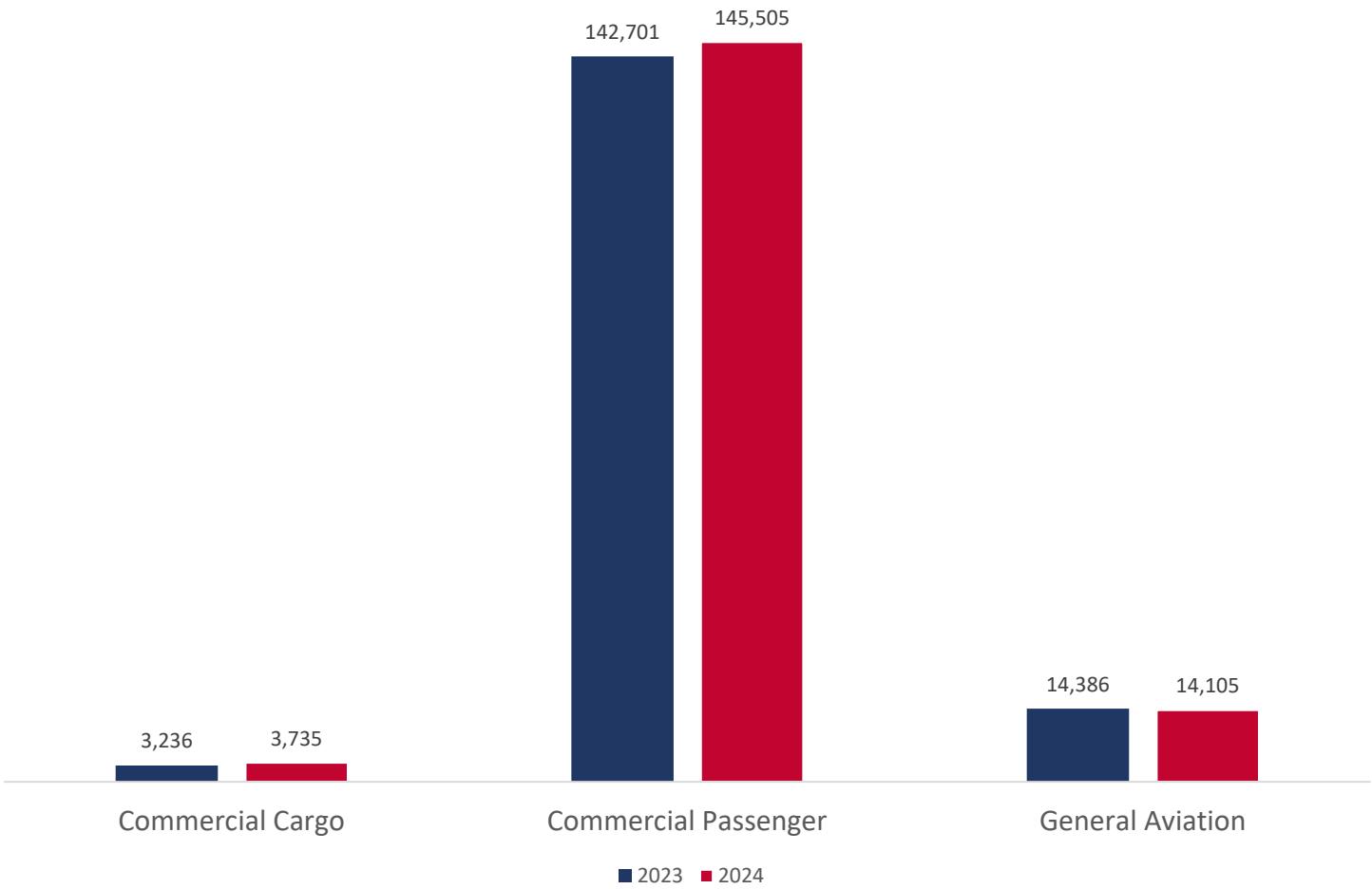
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OPERATIONS BY TYPE OF SERVICE

Commercial Passenger, Commercial Cargo, General Aviation, and Military are the four classifications used in the St. Louis NOMS system. PASSUR Aerospace* also includes an Unknown operations category for aircraft that were not identified either by airline or aircraft equipment type or both. In 2024, Commercial passenger operations accounted for 91% of the total operations as compared to 90.7% in 2023. General Aviation operations, which consist of privately operated business jets, propeller aircraft, and helicopters, made up 8.8% of the total operations in 2024, as compared to 9.1% in 2023. Commercial Cargo operations comprised 2.3% of total operations for 2024, which was an increase from 2.1% in 2023. Total military operations are unknown because most military operations are removed from the data feed by the FAA for security reasons at the request of the Department of Defense.

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Operations by Type of Service in 2023 and 2024



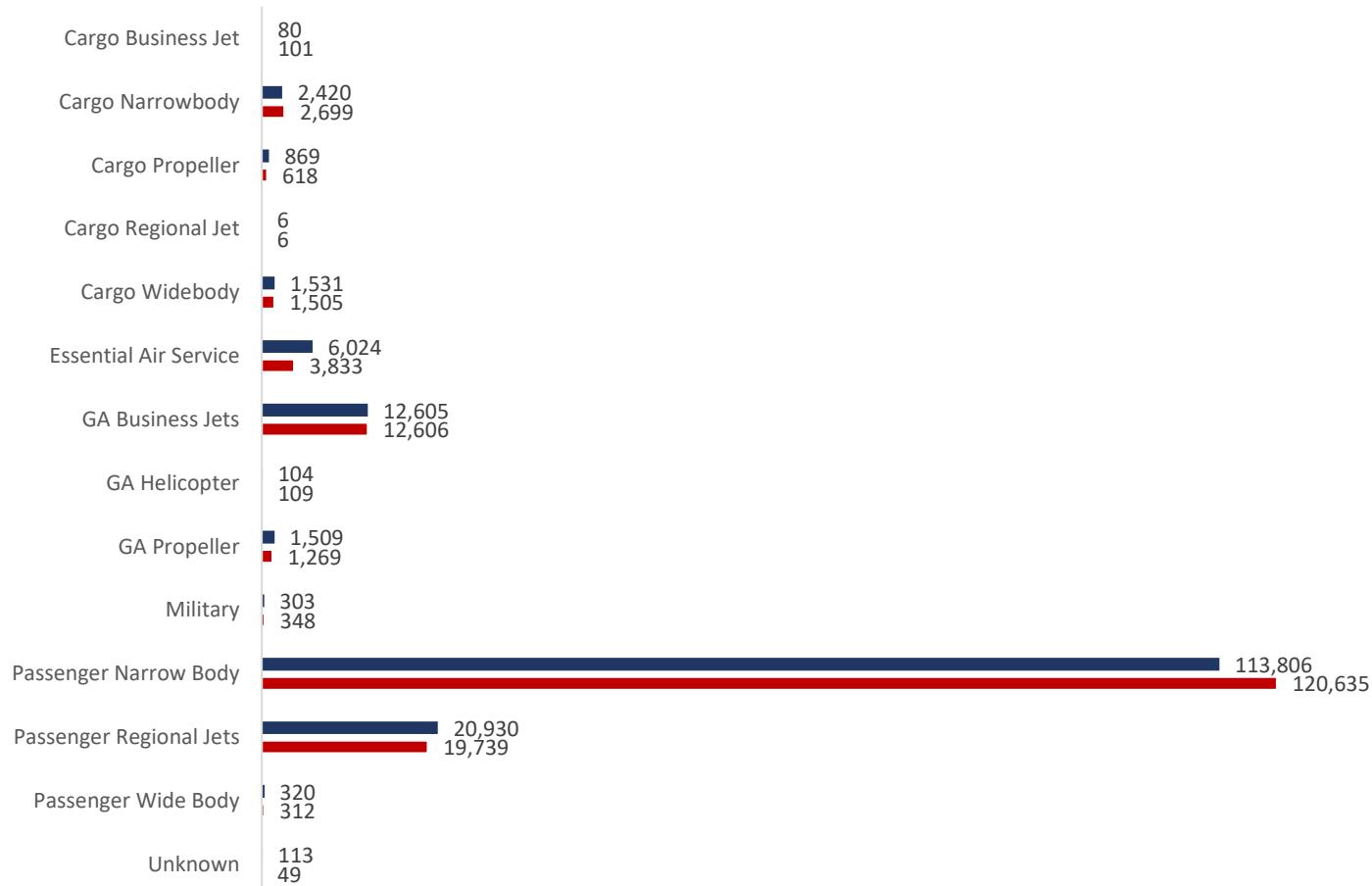
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OPERATIONS BY CATEGORY OF SERVICE

Total operations in 2024 increased 3.3% from 2023, as reported in PASSUR Aerospace data*. Passenger Wide Body operations increased 70.2%, and Passenger Narrow Body operations were up 16.3% in 2023 as compared to 2022. However, Cargo Widebody operations decreased -32.2%, Essential Air Service operations dropped -31.3%, Passenger Regional Jet operations fell -23.2%, Cargo Narrowbody operations decreased -18.6%, General Aviation Propeller decreased by -8.1%, and General Aviation Business Jet operations decreased by -4.9% in 2023 as compared to 2022. Report of Military operations is incomplete because most military operations are removed from the data feed by the FAA for security reasons at the request of the Department of Defense.

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Operations by Category of Service 2023 and 2024



■ 2023 ■ 2024

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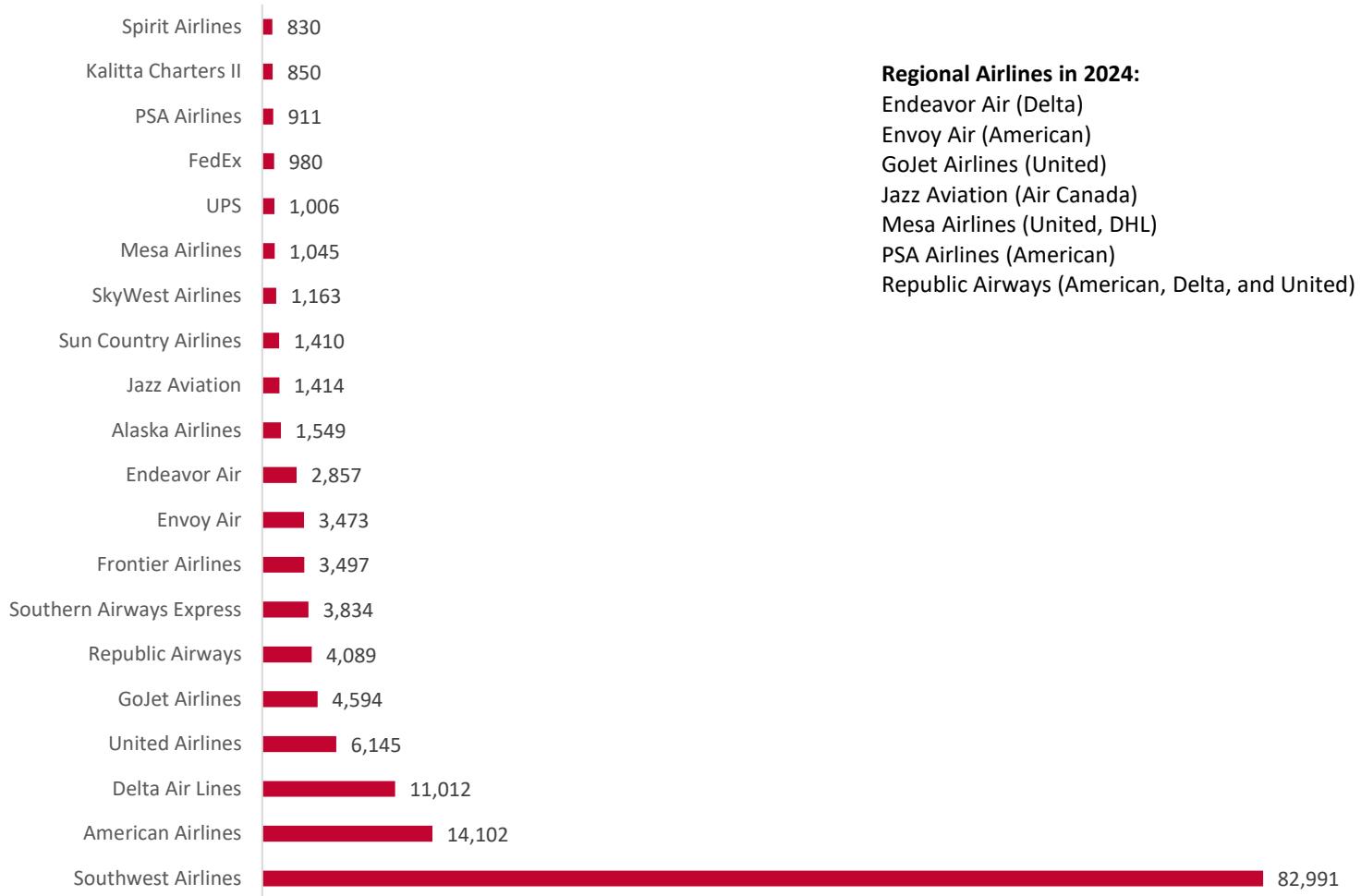
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OPERATIONS BY AIRLINE

The top twenty airlines in 2024, including mainline passenger airlines and their regional airline affiliates, accounted for 90.2% of all operations. Shown in the chart below are the top 20 carriers, comprised of sixteen passenger airlines, three cargo exclusive airlines (FedEx, Kalitta, and UPS), and one Essential Air Service airline (Southern Airways Express). Mainline passenger airlines accounted for 74.5% of the operations by the Top 20 Carriers in 2024. Regional carriers accounted for 12.1%, while cargo airlines accounted for 2.3%. Southwest, Spirit, and Frontier are the only mainline passenger airlines that do not have any regional airline partners at St. Louis Lambert International Airport.

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Operations by the Top 20 Carriers in the 2024



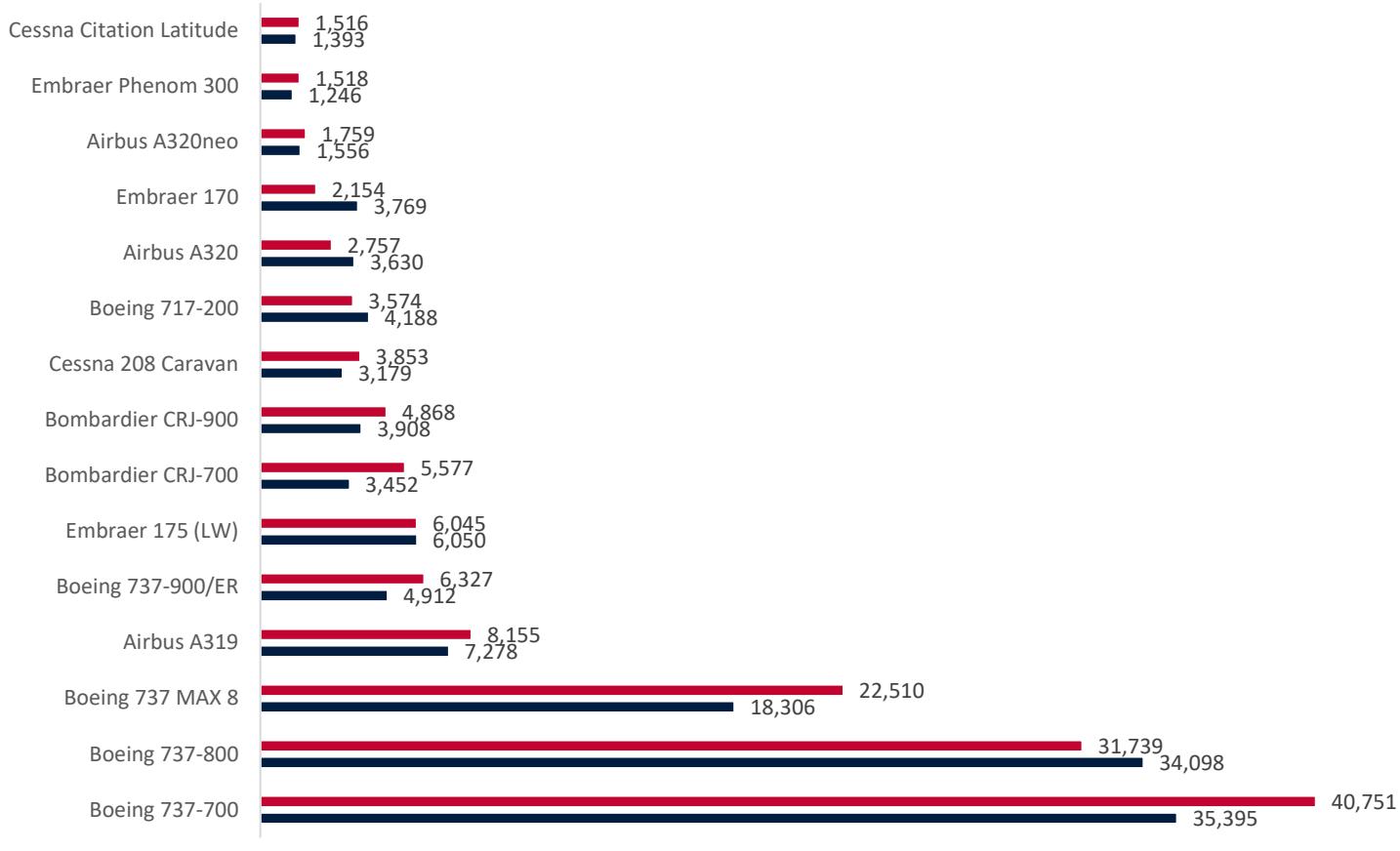
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OPERATIONS BY AIRCRAFT

Shown below are the top 15 aircraft types operating at St. Louis Lambert Airport in 2024. The Boeing 737-700, which is highly utilized by STL's largest airline, Southwest, recorded the most operations with (40,751) -- an increase of 15.1% over 2023. In second position, with 31,739 operations, representing a decrease of 6.9%, was the 737-800. The 737-800 is also heavily utilized by Southwest, as well as American, the second-largest carrier at STL. Total Commercial Passenger operations were up 15.3% in 2024. In 2024 there were 206 unique aircraft types identified that landed at St. Louis Lambert International Airport.

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Operations by Aircraft in 2023 and 2024 15 Aircraft Types Operating at STL

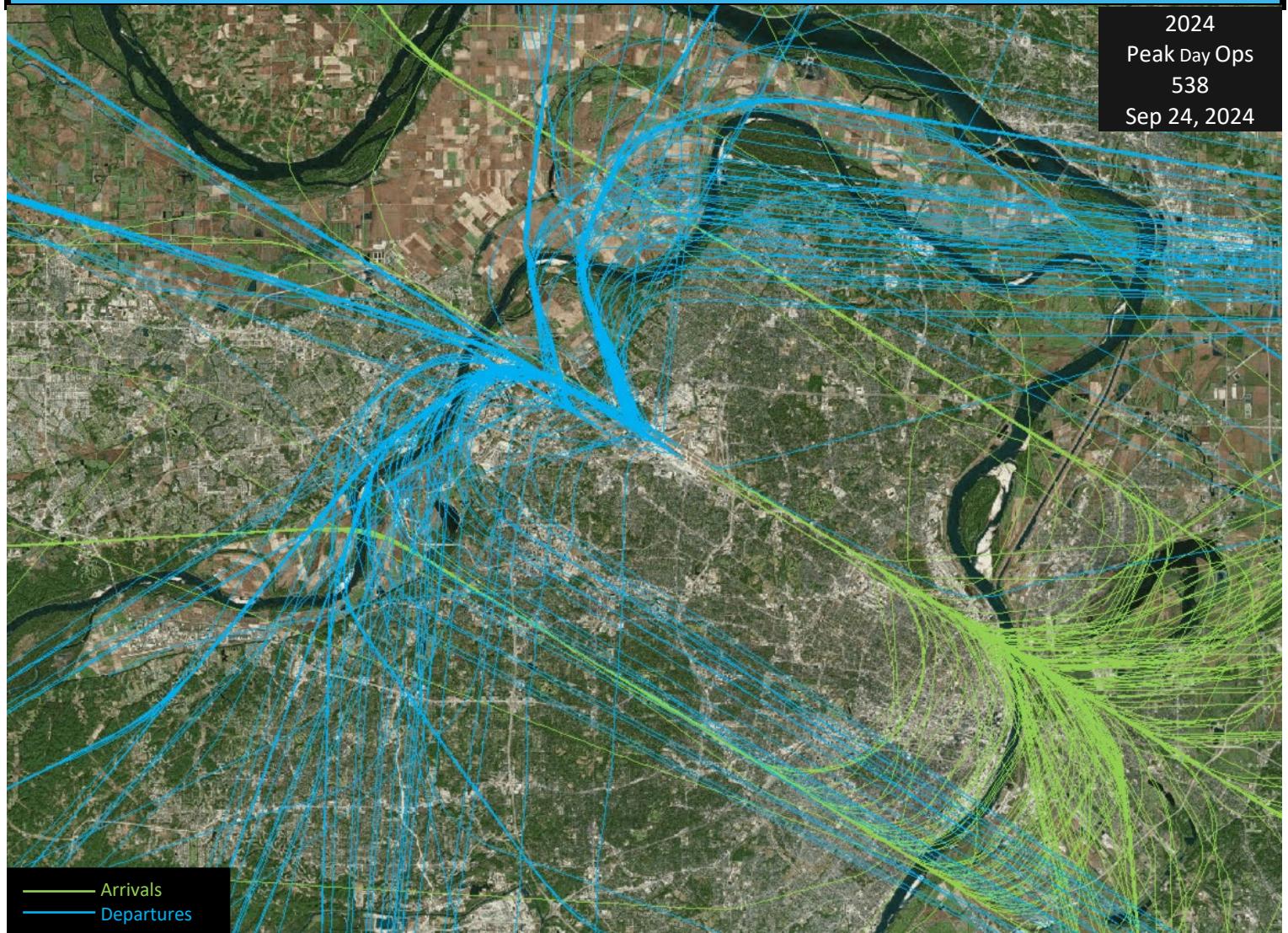


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Peak Day Operations

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